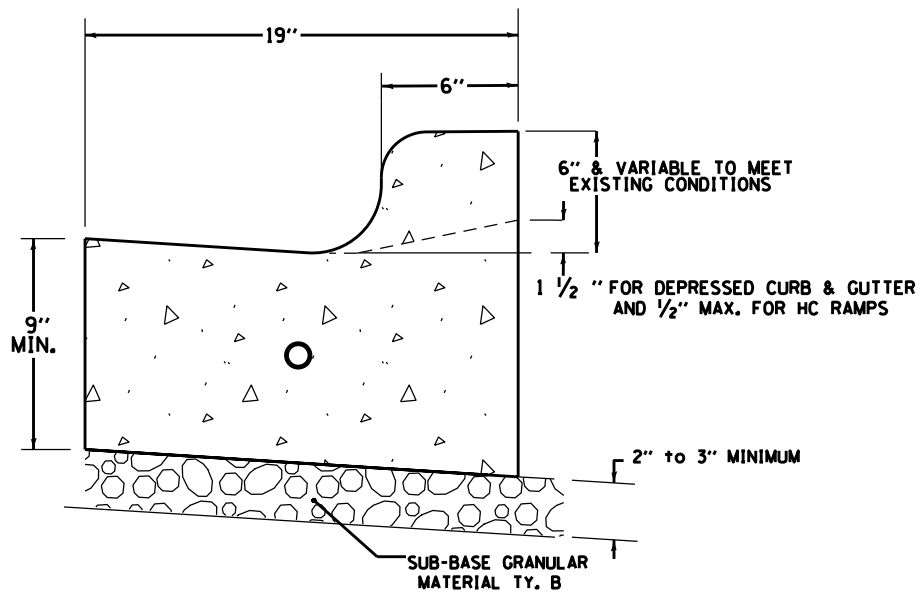




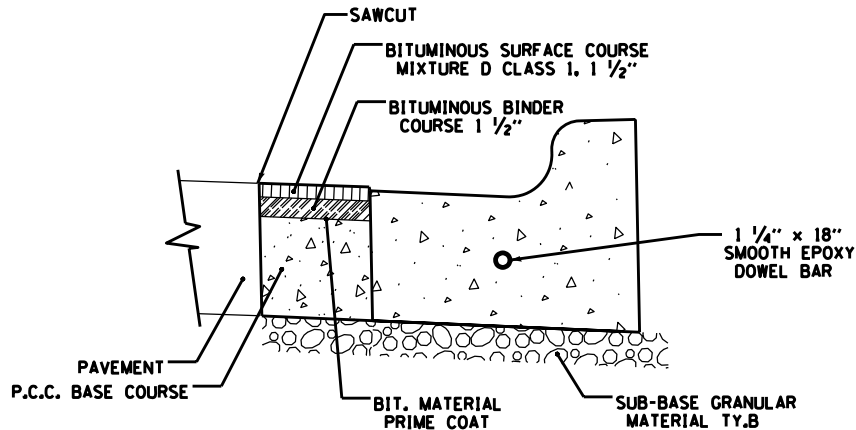
Right of Way

Restoration Standards

Pavement Restoration
Bituminous Pavement Restoration PCC
Pavement Restoration Temporary Patch
Driveway Replacement
Combination Curb & Gutter
Parkway Restoration
Tree Protection



COMBINATION CONCRETE CURB & GUTTER TYPE B-6.12



STREET RESTORATION DETAIL

NOTES:

NOTIFY THE VILLAGE OF OAK PARK, ENGINEERING DIVISION 708-358-5700 AND J.U.L.I.E. 800-892-0123 48 HOURS IN ADVANCE OF WORK

COMBINATION CURB AND GUTTER SHALL BE INSTALLED ACCORDING TO IDOT STANDARD 606001-02.

MATERIALS

- CONCRETE CLASS "2" (6.2 BAG MIX) (22 HOURS CURING PERIOD)
- PREFORMED EXPANSION JOINT FILLER

EXPANSION 1" BETWEEN CURB & GUTTER AND SIDEWALKS/DRIVES

1" TO CONFORM TO CROSS SECTION OF CURB & GUTTER WITH DOWEL BAR WWW - 18" COATED W/ PINCH CAP

CONTRACTION 1/8" SAW-TROWLED

CONSTRUCTION 1/2" 2-"#4 REBARS

•SEE COMBINATION CURB & GUTTER DETAIL #2 FOR ADDITIONAL INFORMATION

SLOPE

GUTTER FLAG TO HAVE 3/4" PER FOOT SIDE SLOPE

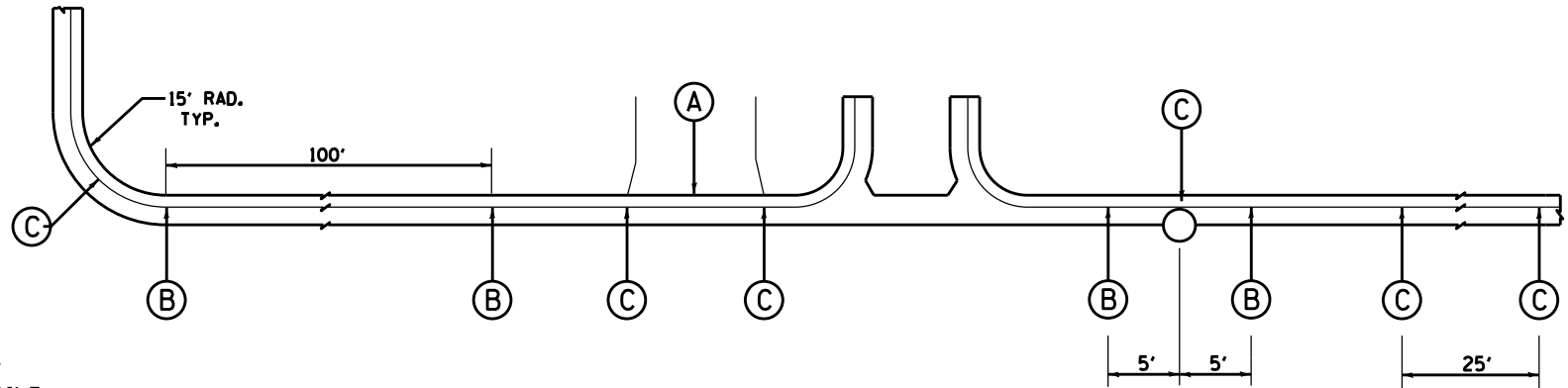
GUTTER FLOW LINE TO HAVE 0.4% SLOPE PER FOOT (MINIMUM)

TRAFFIC CONTROL

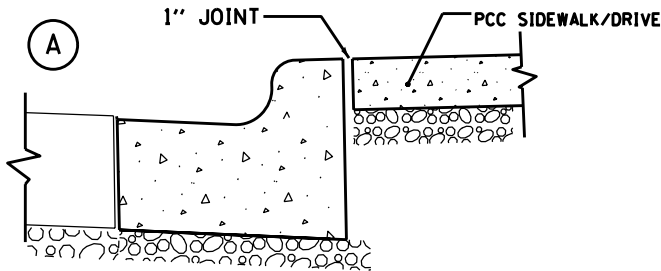
PROVIDE AND MAINTAIN APPROVED TRAFFIC CONTROL DEVICES APPLICABLE TO WORK. "FLASHING BARRICADES SHALL BE USED FOR ALL NIGHT OPERATIONS"

•SEE CONSTRUCTION DETAIL 444 FOR ADDITIONAL INFORMATION

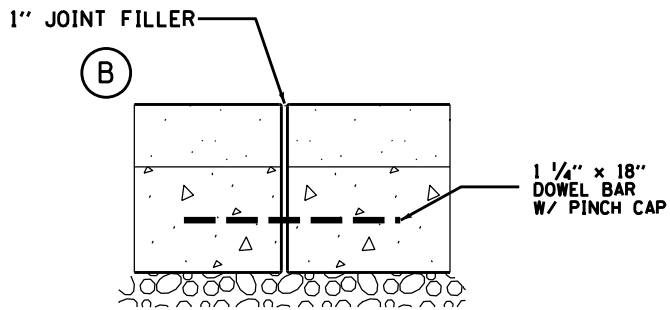
ADJACENT TO FLEXIBLE PAVEMENT



EXPANSION JOINTS

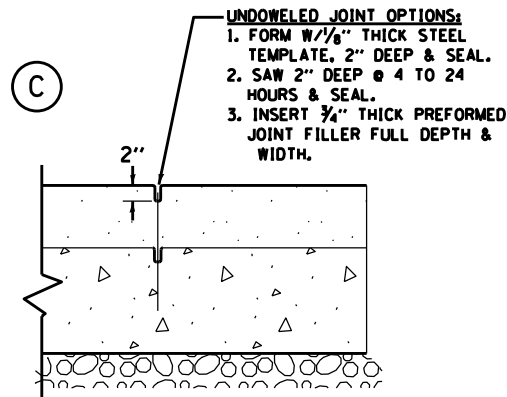


CURB & GUTTER ADJACENT TO SIDEWALK/DRIVE



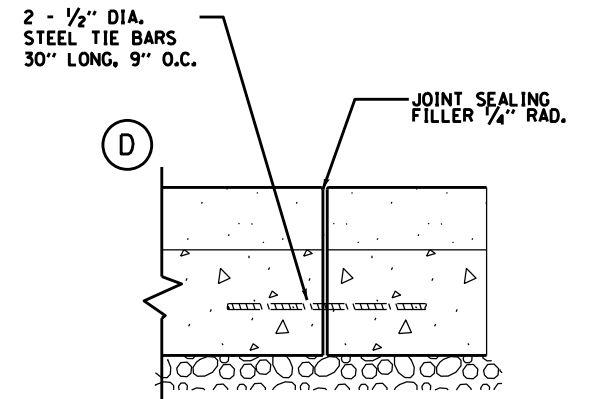
CURB & GUTTER AT P.C. AND 100' INTERVALS

CONTRACTION JOINT

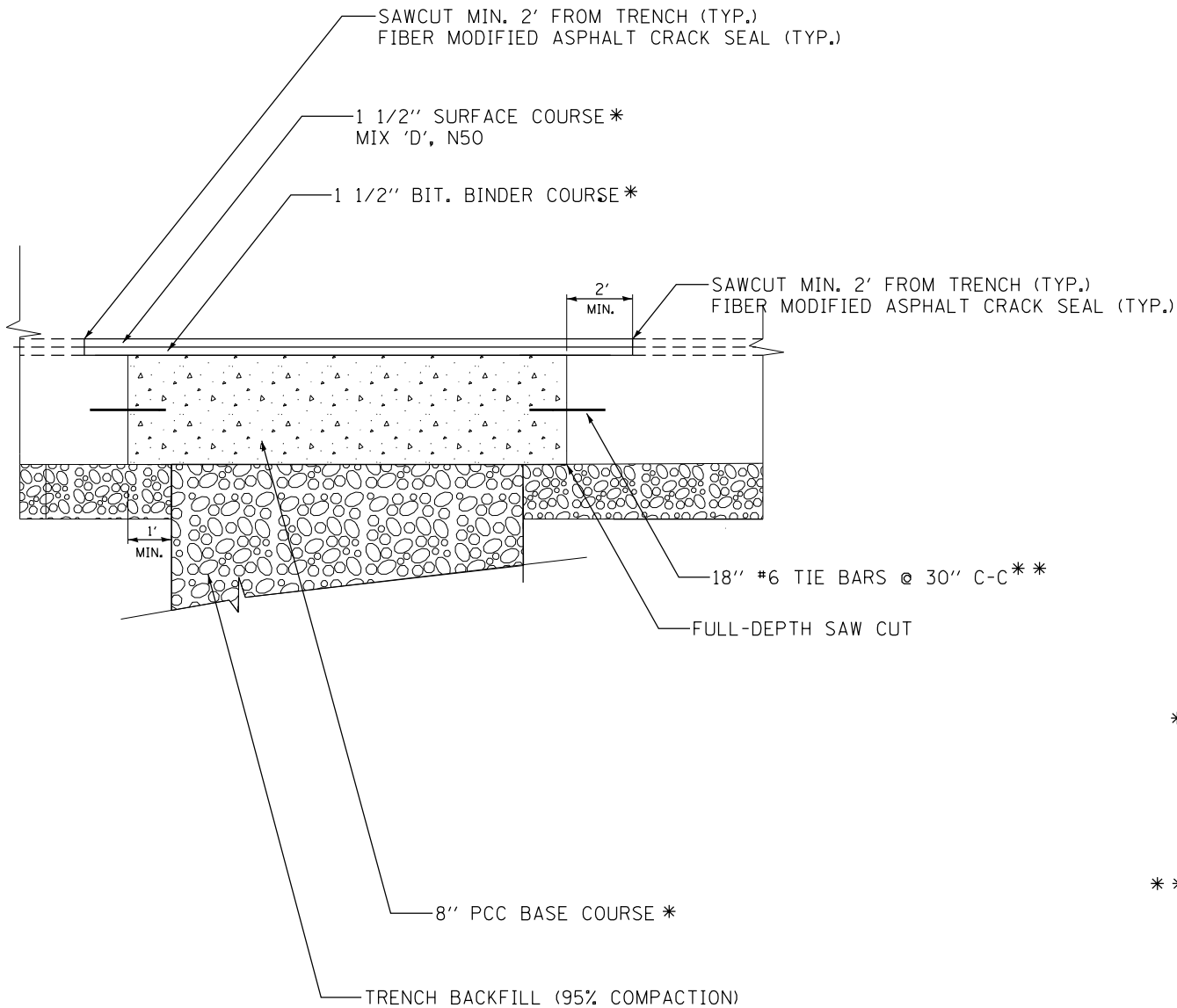


CURB & GUTTER AT DRAINAGE CASTING AND 25' INTERVALS

CONSTRUCTION JOINT



CURB & GUTTER AT NECESSARY LOCATIONS



NOTES:

FIBER MODIFIED ASPHALT CRACK SEAL
REQUIRED FOR ALL SURFACE JOINTS

NOTIFY THE VILLAGE OF OAK PARK, ENGINEERING
DIVISION 708-358-5700 AND J.U.L.I.E. 800-892-0123
48 HOURS IN ADVANCE OF WORK

ALL MATERIALS AND METHODS SHALL MEET THE
REQUIREMENTS OF THE LATEST "STANDARD
SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION"
OF THE STATE OF ILLINOIS, UNLESS SPECIFICALLY
NOTED IN THIS STANDARD.

UNSUITABLE SUB-BASE MATERIAL TO BE REPLACED
WITH 4" SUB-BASE MATERIAL, TYPE B AS DIRECTED
BY VOP ENGINEERING

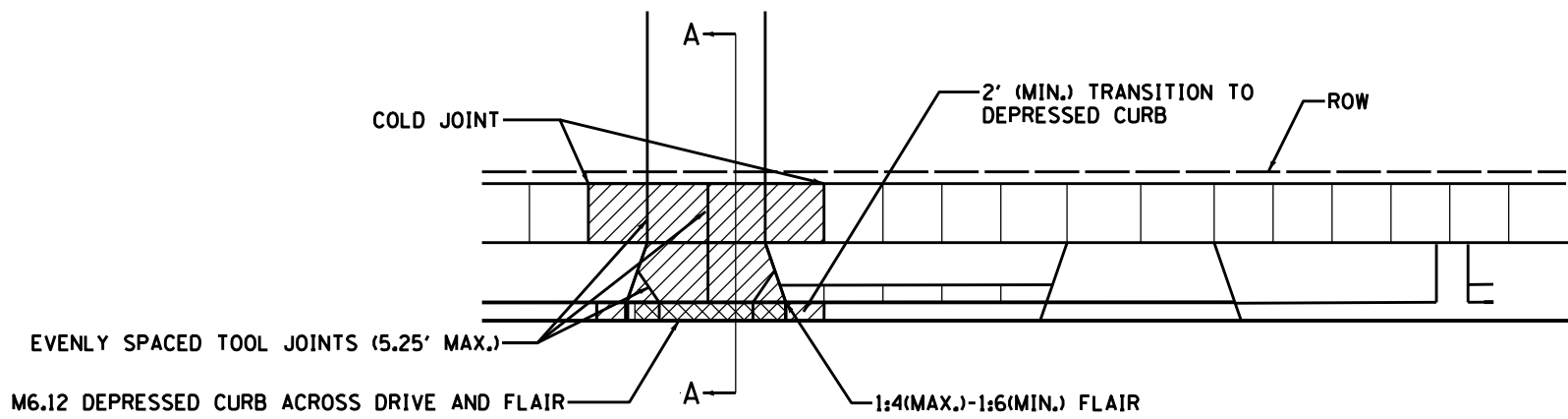
IN LIEU OF BINDER COURSE 3" OF SURFACE COURSE
MAY BE USED IF THE TOTAL PATCH AREA IS LESS
THAN 200 SQ. FT

IF THE PERMANENT PATCH CANNOT BE PLACED WITHIN
FIVE BUSINESS DAYS AFTER COMPLETION OF THE WORK,
A TEMPORARY PATCH SHALL BE PLACED.

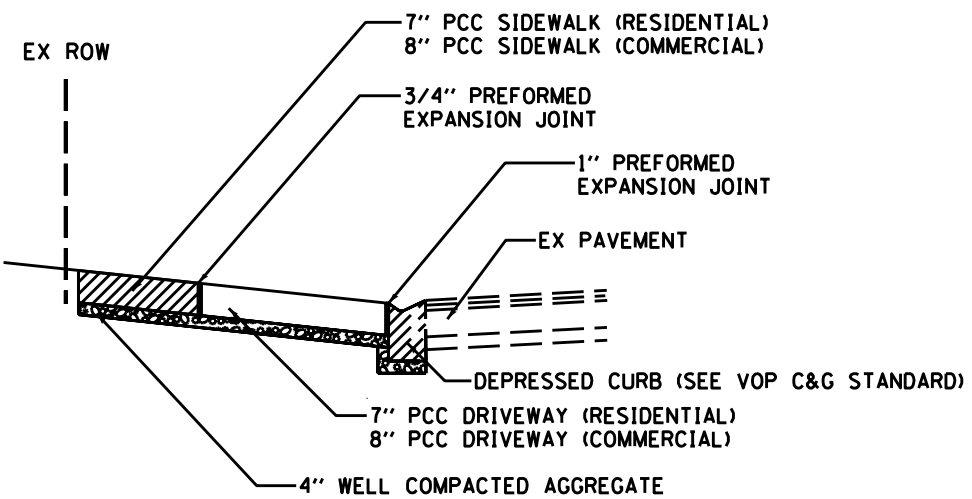
* PATCH SHALL MATCH EXISTING PAVEMENT
SECTION THICKNESS AND CROSS SECTION
IF EXISTING PAVEMENTS ARE THICKER THAN SHOWN.
REQUIRED DEPTHS FOR MINIMUM THICKNESSES:
SURFACE COURSE: 2"
BINDER COURSE: NOT REQUIRED
PCC BASE COURSE: 8"

** TIE BARS REQUIRED WHERE ADJACENT BASE
COURSE IS PORTLAND CEMENT CONCRETE

Date: 3/30/2011



M6.12 DEPRESSED CURB ACROSS DRIVE AND FLAIR



SECTION A-A

NOTES:

THE CONTRACTOR SHALL CONSTRUCT THE NEW CURB WITH THE USE OF A FRONT BOARD TO PROVIDE A STRAIGHT CONCRETE EDGE MEETING THE EXISTING ASPHALT STREET SURFACE. THE VOID SHALL BE FILLED WITH CONCRETE AND ASPHALT AS REQUIRED.

CURBS SHALL BE POURED SEPARATELY FROM THE DRIVEWAYS.

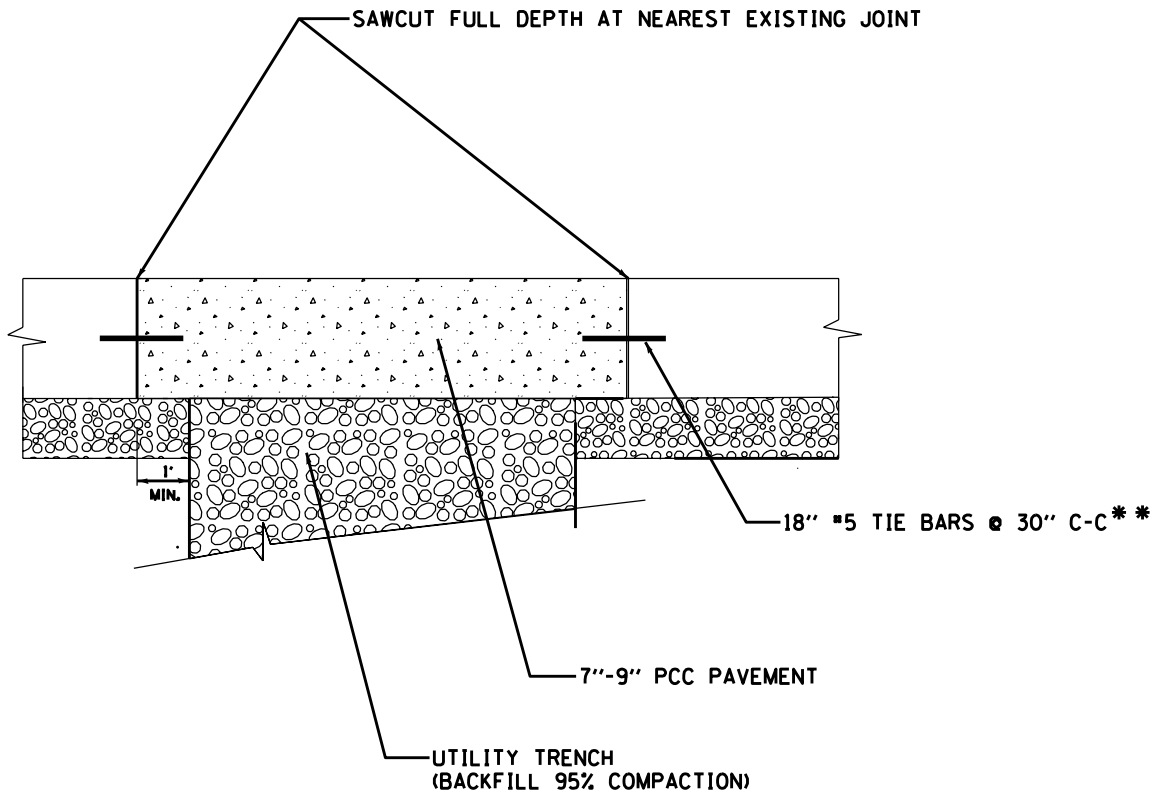
SIDEWALK ADJACENT TO DRIVEWAYS SHALL BE THE SAME DEPTH AS THE DRIVEWAY

CURB LIMITS SHALL EXTEND TO THE NEAREST EXISTING CURB JOINT ON EACH SIDE OF THE DRIVEWAY OPENING. IF THE NEAREST JOINT IS LESS THAN 2 FT. FROM THE DRIVEWAY OPENING, THE LIMIT SHALL EXTEND TO THE NEXT CLOSEST JOINT.

SIDEWALK REPLACEMENT LIMITS SHALL EXTEND TO THE NEAREST EXISTING JOINT. NO PARTIAL SIDEWALK SLAB REPLACEMENTS ARE ALLOWED.

THE DEPTH OF THE TOOL JOINTS SHALL BE 1/4 THE DEPTH OF THE SIDEWALK.

STREET PAVEMENT DAMAGED DURING RECONSTRUCTION SHALL BE CLEANLY SAWCUT AND REPLACED.



NOTES:

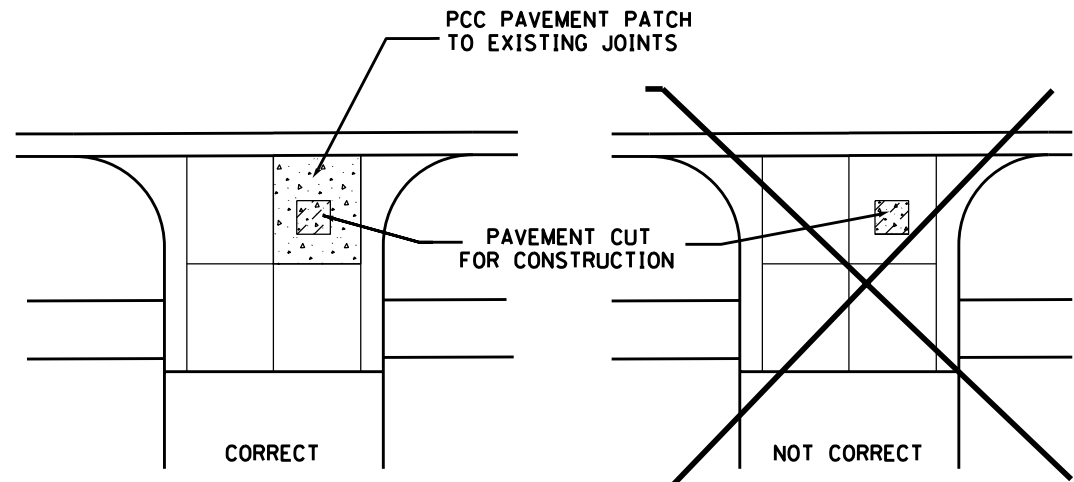
NOTIFY THE VILLAGE OF OAK PARK, ENGINEERING DIVISION 708-358-5700 AND J.U.L.I.E. 800-892-0123 48 HOURS IN ADVANCE OF WORK

ALL MATERIALS AND METHODS SHALL MEET THE REQUIREMENTS OF THE LATEST "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" OF THE STATE OF ILLINOIS, UNLESS SPECIFICALLY NOTED IN THIS STANDARD.

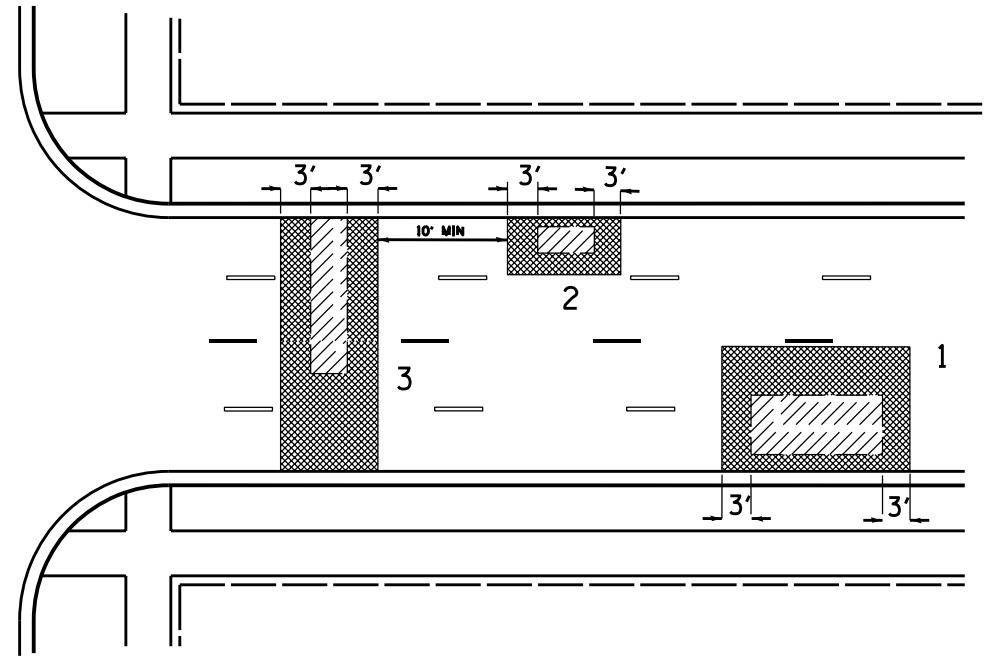
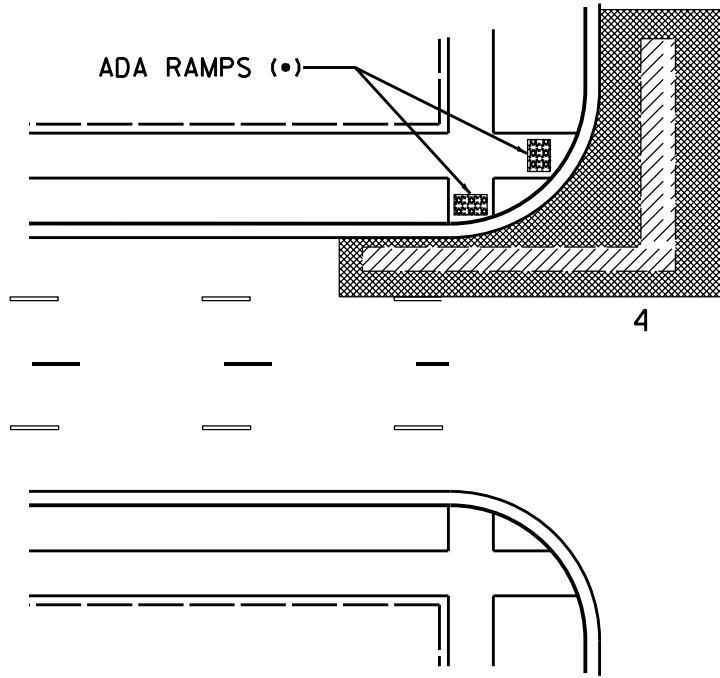
UNSUITABLE SUB-BASE MATERIAL TO BE REPLACED WITH 4" SUB-BASE MATERIAL, TYPE B AS DIRECTED BY VOP ENGINEERING

PCC PAVEMENT - INCLUDING ALLEYS - SHALL BE SAWCUT FULL-DEPTH AT EXISTING JOINTS. SMALLER PATCHES WILL NOT BE ALLOWED. (SEE DRAWINGS BELOW)

IF THE PERMANENT PATCH CANNOT BE PLACED WITHIN FIVE BUSINESS DAYS AFTER COMPLETION OF THE WORK, A TEMPORARY PATCH SHALL BE PLACED.



STREET CUTS AND ASPHALT RESTORATION REQUIREMENTS



LEGEND



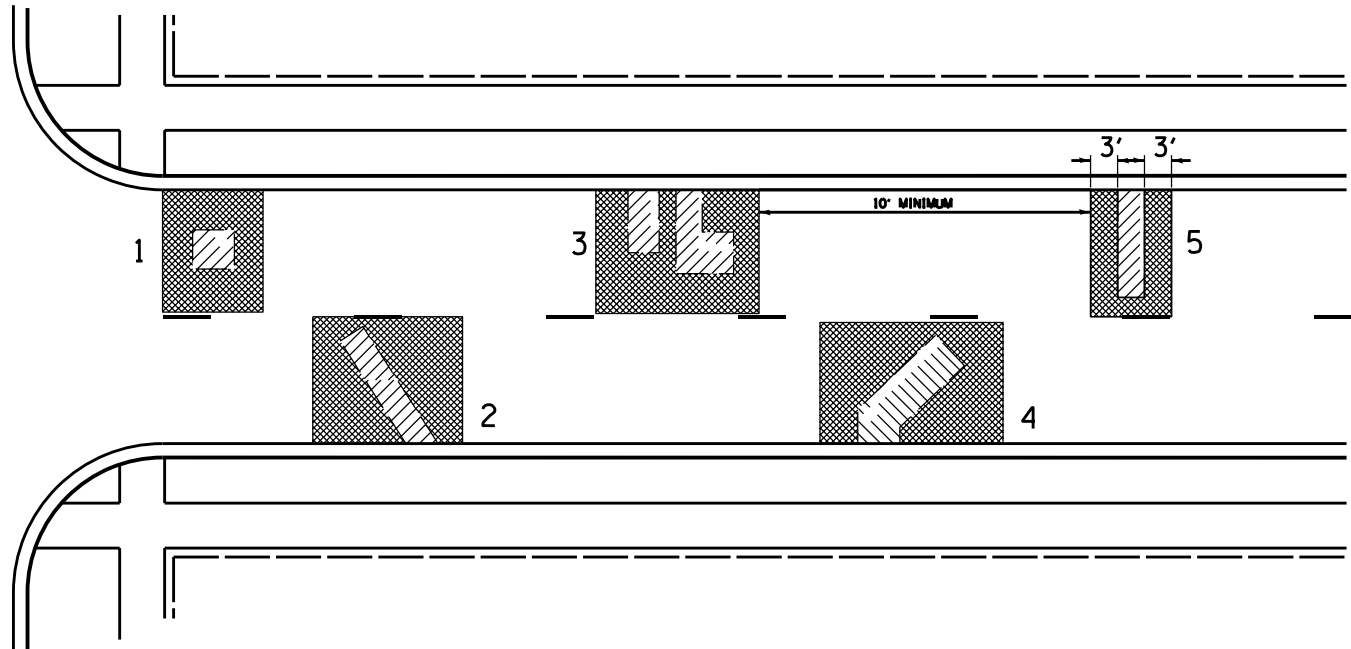
UTILITY/STREET CUT



VOP STANDARD FOR ASPHALT RESTORATION

- (1) 1/2 WIDTH LONGITUDINAL ASPHALT RESTORATION
- (2) 1/4 WIDTH LONGITUDINAL ASPHALT RESTORATION
- (3) FULL WIDTH ASPHALT RESTORATION
- (4) COMB. 1/4 & 1/2 ANGULAR ASPHALT RESTORATION (INCL. HC RAMPS)

STREET CUTS AND ASPHALT RESTORATION REQUIREMENTS



LEGEND



UTILITY/STREET CUT



VOP STANDARD FOR ASPHALT RESTORATION

- (1) 1/2 WIDTH LATERAL ASPHALT RESTORATION
- (2) 1/2 WIDTH LONGITUDINAL ASPHALT RESTORATION
- (3) MULTIPLE CUT ASPHALT RESTORATION
- (4) 1/2 WIDTH LONGITUDINAL ASPHALT RESTORATION
- (5) 1/2 WIDTH LATERAL ASPHALT RESTORATION

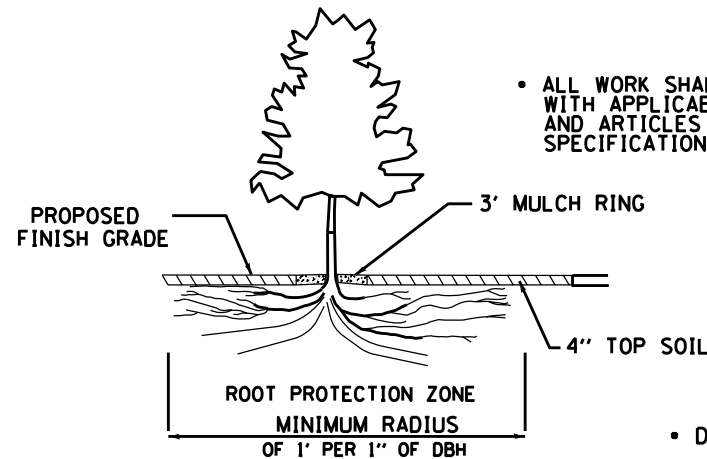
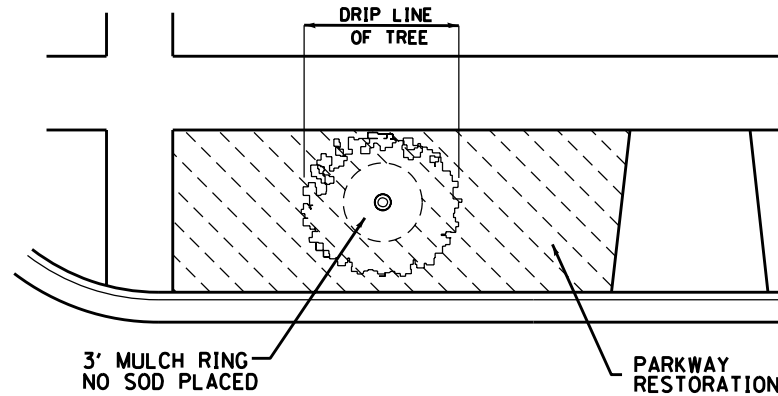
SOD REMOVAL

- SOD REMOVAL BY BACKHOE OR GRADE-ALL TYPE EQUIPMENT WILL NOT BE PERMITTED WITHIN THE ROOT PROTECTION ZONE.
- AN APPROVED MECHANICAL SOD CUTTER SHALL REMOVE SOD WITHIN THE DRIPLINE OF PARKWAY TREES.

PARKWAY RESTORATION

PARKWAY RESTORATION IS REQUIRED FOR ALL AREAS DAMAGED DURING CONSTRUCTION, (BEHIND ALL NEW CURBS AND ALONG NEWLY CONSTRUCTED DRIVEWAYS AND SIDEWALKS) EXTENDING TO THE CLOSEST EXISTING CONCRETE EDGES.

- OTHER AREAS USED FOR STORAGE OF MATERIALS, SHOWN OF PLANS AND/OR DIRECTED BY THE ENGINEER WILL ALSO BE RESTORED.
- AREAS DAMAGED BY CONSTRUCTION EQUIPMENT OR STORAGE OF MATERIALS SHALL BE RAKED TO REMOVE ANY FOREIGN MATERIALS AND BE CULTIVATED TO A DEPTH OF 4" BY ROTO-TILLING.
- EXCAVATED AREAS SHALL BE BACKFILLED UP TO 4" BELOW THE PROPOSED GRADE WITH SUITABLE MATERIALS FROM THE JOB SITE OR THOSE HAULED IN BY THE CONTRACTOR.
 - ALL SUCH MATERIALS SHALL CONSIST OF CLAY, FINE GRANULAR MATERIALS OR OTHER MATERIALS APPROVED BY THE ENGINEER.
 - NO MATERIALS CONTAINING BROKEN CONCRETE OR ROCKS AND STONES LARGER THAN 1" IN GRADATION WILL BE ALLOWED.
 - THE BALANCE OF FILL UP TO THE PROPOSED GRADE LINE SHALL BE OF TOPSOIL.
- THE PROPOSED GRADE OF THE PARKWAY SHALL BE A TRUE LINE FROM THE SIDEWALK TO THE TOP OF THE ADJACENT STREET CURB.
- AFTER THE TOP SOIL IS GRADED, IT SHALL BE ROLLED WITH HAND ROLLERS TO A FIRM DENSITY AND FERTILIZER SHALL BE APPLIED AT A RATE OF 180 lbs. PER ACRE BY MEANS OF MECHANICAL SPREADER.
- WHERE SOD IS NOT FEASIBLE, THE ENGINEER MAY REQUEST MULCH PLACED OVER THE REQUIRED 4" OF TOPSOIL.
- THE BASE OF EACH PARKWAY TREE WILL HAVE 3' MINIMUM RING LEFT UNSODDED.
 - THE CONTRACTOR WILL MULCH THIS RING WITH AN APPROVED MATERIAL AT A DEPTH NOT TO EXCEED 3".
 - NO MULCH WILL BE PLACED DIRECTLY AT THE BASE OF THE TREE OR MOUNDED AROUND THE TRUNK.



• ALL WORK SHALL BE ACCOMPLISHED IN ACCORDANCE WITH APPLICABLE SPECIFICATIONS OF SECTION 252 AND ARTICLES 1081.03 AND 1081.08 OF THE STANDARD SPECIFICATIONS.

• DBH=TREE DIA. MEASURED 4.5' ABOVE GROUND

CONSTRUCTION TREE DAMAGE AND THE VILLAGE REPLACEMENT POLICY

THE FORESTRY SUPERVISOR OR HIS DESIGNEE WILL CONDUCT PERIODIC INSPECTIONS DURING THE COURSE OF CONSTRUCTION. IF IT IS DETERMINED THAT DAMAGE HAS OCCURRED AS A RESULT OF CONSTRUCTION ACTIVITY, THE FOLLOWING WILL APPLY:

- ALL ASSOCIATED COSTS, INCLUDING REPAIR AND REIMBURSEMENT FOR DAMAGES ARE AT THE CONTRACTOR'S EXPENSE.
- VALUE OF PARKWAY TREES WILL BE CALCULATED AT THE ISA EVALUATION BASED ON THE LATEST VILLAGE OF OAK PARK TREE INVENTORY.
- REIMBURSEMENT FOR DAMAGES WILL BE ASSESSED AS A PERCENTAGE OF THE TOTAL VALUE BASED ON THE AMOUNT OF DAMAGE TO THE TREE.
- REPAIR WORK WILL BE CHARGED AT A TIME AND MATERIAL RATE BASED ON NECESSARY LABOR AND EQUIPMENT COSTS.
- THE CONTRACTOR WILL REPLACE A PARKWAY TREE DETERMINED BY THE FORESTRY SUPERVISOR OR HIS DESIGNEE TO BE DAMAGED BEYOND REPAIR AS A RESULT OF CONSTRUCTION ACTIVITY.
- SHALL BE REPLACED ON AN INCH FOR DIA. INCH BASIS. (MEAS. AT DBH)

ROOT PRUNING SPECIFICATION

- ROOT PRUNING IS MANDATORY FOR ANY EXCAVATION THAT OCCURS IN THE PUBLIC RIGHT-OF-WAY WITHIN THE DRIP LINE OF A PUBLIC TREE.
- ROOT PRUNING SHALL BE ACCOMPLISHED USING AN APPROVED MECHANICAL ROOT PRUNING DEVICE WHICH CUTS TO A MIN. DEPTH OF EIGHTEEN INCHES (18").
- BEFORE THE BEGINNING OF CONSTRUCTION OR DELIVERY OF MATERIALS, ROOT PRUNING SHALL BE COMPLETED AND BACKFILLED.
- ROOT PRUNING MUST OCCUR NO MORE THAN SIX INCHES (6") OUTSIDE THE LIMITS OF EXCAVATION AND BE CONTINUOUS WITHIN THE DRIP LINE OF THE TREE.
- ANY ROOTS GREATER THAN 1" IN DIAMETER ENCOUNTERED BELOW A DEPTH OF 24" SHALL BE TRIMMED FLUSH WITH A SHARP SAW TO ELIMINATE RAGGED EDGES.
- ROOT PRUNING CAN OCCUR NO EARLIER THAN 1 MONTH PRIOR TO BEGINNING OF EXCAVATION.
- THE ROOT PRUNING TRENCH SHALL BE BACKFILLED WITHIN 48 HOURS OF EXCAVATION.

• AT THE DISCRETION OF THE VILLAGE FORESTER ROOT PRUNING AND TREE PROTECTION FENCING MEASURES MAY NOT BE REQUIRED FOR SOME NON INVASIVE CONSTRUCTION ACTIVITIES. (eg. REMOVE AND REPLACE-SIDEWALK)

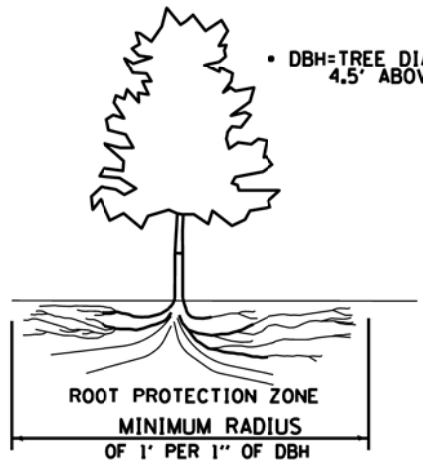
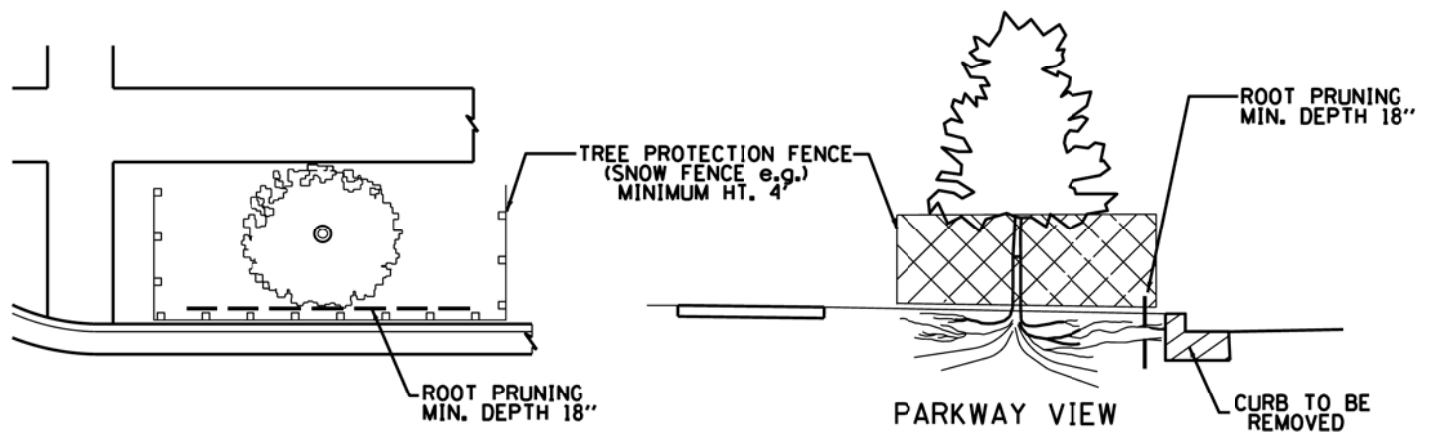
• SEE PARKWAY RESTORATION STANDARD FOR VILLAGE POLICY ON CONSTRUCTION TREE DAMAGE AND REPLACEMENT.

TOP PRUNING

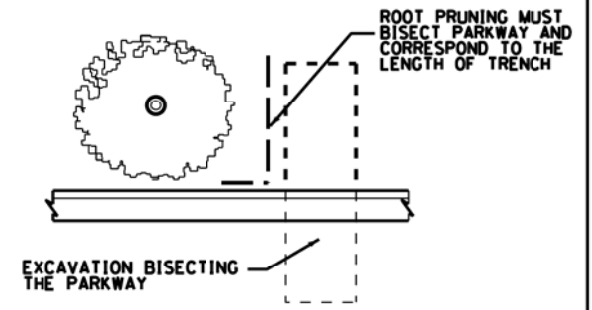
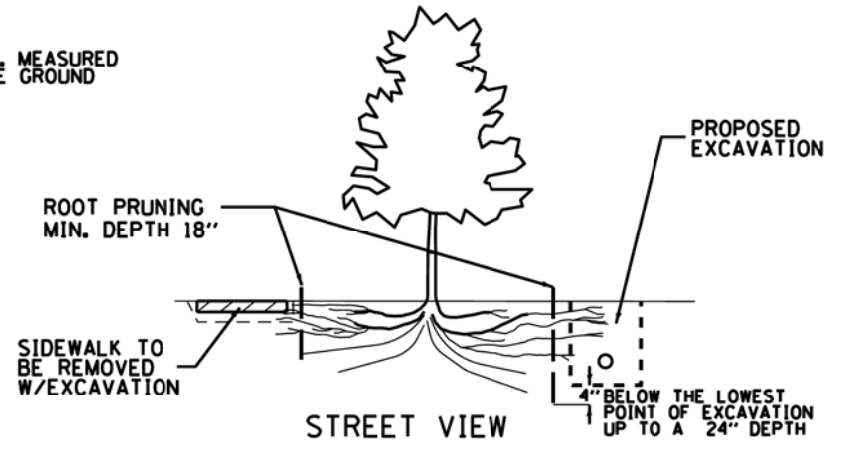
- AT THE DIRECTION OF A FORESTRY SUPERVISOR:
 - TREES MAY BE PRUNED TO ALLOW EQUIPMENT ACCESS.
 - LIMBS MAY BE TEMPORARILY TIED BACK WITH APPROVED MATERIAL.
- CROWN REDUCTION MAY NOT EXCEED 25%.
- TOP PRUNING MUST BE COMPLETED BY THE BEGINNING OF CONSTRUCTION.

ROOT ZONE PROTECTION

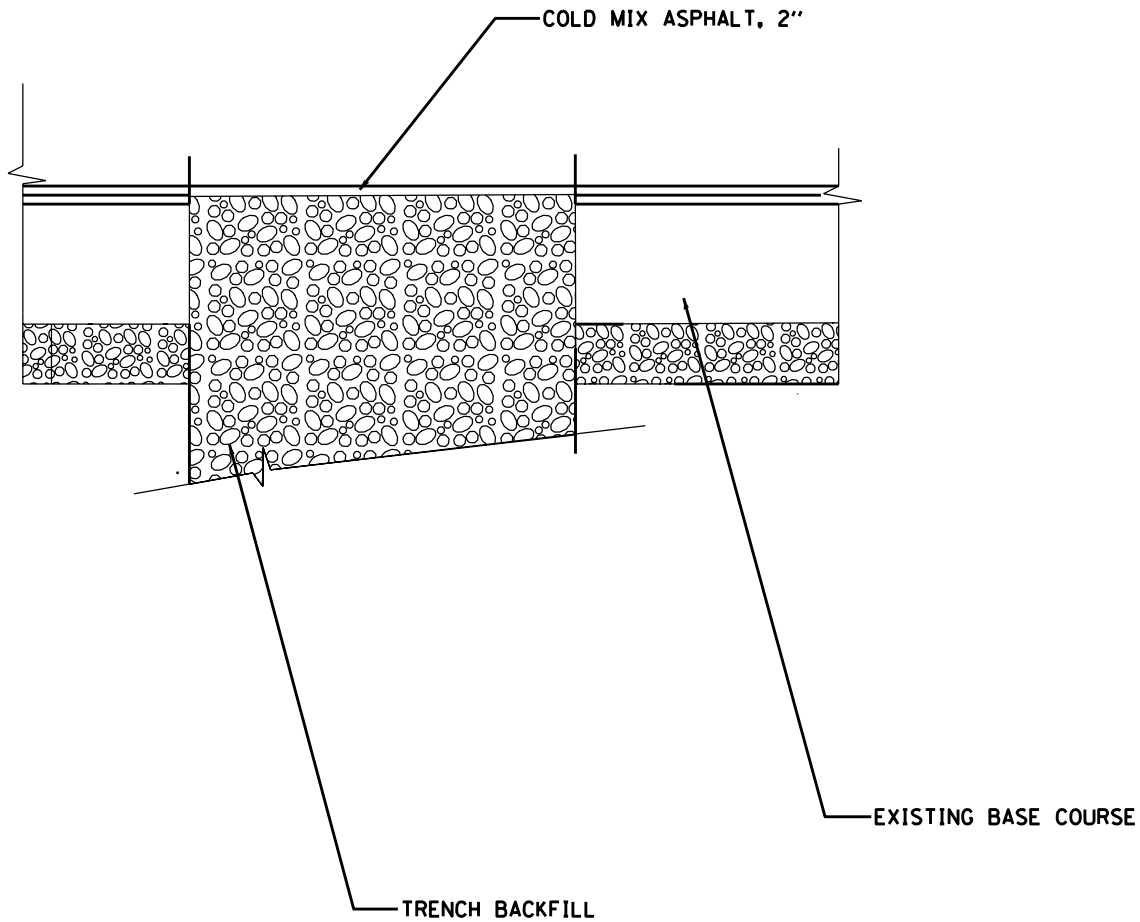
- CONSTRUCTION DEBRIS, MATERIAL, FITTINGS, EQUIPMENT, ETC. SHALL NOT BE PLACED WITHIN THE ROOT PROTECTION ZONE.
- METAL CHANNEL POSTS OR APPROVED EQUIVALENT POSTS SHOULD BE PLACED AT A MAX. OF EVERY EIGHT FEET.
- POLES MUST BE AT LEAST SIX FEET IN LENGTH, TWO FEET OF WHICH MUST BE SET IN THE GROUND.
- IF CONSTRUCTION EQUIPMENT MUST CROSS THE PARKWAY OUTSIDE THE ROOT PROTECTION AREA, THE PARKWAY MUST BE PROTECTED WITH A SIX INCH LAYER OF WOOD MULCH OR CHIPS, OR ONE SHEETS OF PLYWOOD OR SIMILAR APPROVED MATERIAL.



• DBH=TREE DIA. MEASURED 4.5' ABOVE GROUND



EXCAVATION BISECTING THE PARKWAY



NOTES:

STREET OPENINGS MUST BE CLOSED AS SOON AS POSSIBLE AFTER THE COMPLETION OF THE UTILITY WORK.

WHEN THE PERMANENT PATCH CANNOT BE INSTALLED WITHIN FIVE BUSINESS DAYS AFTER COMPLETION OF THE TRENCH WORK, A TEMPORARY PATCH MUST BE PLACED.

THE ENGINEERING DIVISION MUST BE GIVEN WRITTEN NOTIFICATION OF THE DELAY AND AN ESTIMATED COMPLETION DATE FOR THE PERMANENT PATCH INSTALLATION.

ALL MATERIALS AND METHODS SHALL MEET THE REQUIREMENTS OF THE LATEST "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" OF THE STATE OF ILLINOIS, UNLESS SPECIFICALLY NOTED IN THIS STANDARD.